

Department of Planning and Zoning

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MEMORANDUM

To: Development Review Board
From: Mary O'Neil, AICP, Senior Planner *monell*
Date: August 5, 2014
RE: ZP 14-1311SP, 86 Main Street

Note: These are staff comments only. Decisions on projects are made by the Development Review Board, which may approve, deny, table or modify any project. THE APPLICANT OR REPRESENTATIVE MUST ATTEND THE MEETING.

File: ZP 14-1311SP

Location: 86 Main Street
(northwest corner of Main and Pine Streets)

Zone: D **Ward:** 3

Date application accepted:
June 27, 2014

Applicant/ Owner: Tyler Scott
/ Grandview Farms, Inc.

Request: Sketch Plan review
for proposed twenty three unit
apartment building with office
space, 8 internal parking spaces.

Background:

- **Zoning Permit 12-0168SN;** three new signs for Green Mountain Rehab and Sports Medicine. August, 2011.
- **Zoning Permit 11-0524CA;** change use of existing office space to medical office with retail. December 2010.
- **Zoning Permit 10-0560CA;** installation of rooftop mechanical equipment. December, 2009.
- **Zoning Permit 10-0446CA;** replace all windows on second floor, replace awning, install new exterior lighting. November 2009.
- **Zoning Permit 10-0272SN;** one free standing non illuminated metal sign for Chop Shop Hair Design. September 2009.
- **Zoning Permit 10-0273CA;** Change of use for first floor from office and retail to salon with retail. September 2009.



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- **Zoning Permit 06-178CA**; Remove and replace wooden ramp and roof. Construct new covered ramp and stair entrance to upper level. September 2005.
- **Zoning Permit 00-605**; Parallel sign for existing office space. June 2000.
- **Zoning Permit 00-066**; installation of internally illuminated freestanding sign for office/furniture retail use. August 1999.
- **Zoning Permit 99-474**; installation of an awning without signage. April 1999.
- **Zoning Permit 99-378**; change of use for first floor from elderly daycare to offices with display area for special order office furniture. Includes some exterior changes. March 1999.
- **Zoning Permit 95-245**; installation of two externally illuminated parallel signs for the exiting second floor office space. December 1994.
- **Zoning Permit 92-408**; installation of gyroscope exercise machine in Senior Citizen Center parking lot on weekends. June 1991.
- **Non Applicability of Zoning Permit Requirements**; utilize 2,900 sq. ft. of existing general office space for the same use being general office space. August 1994.
- **Zoning Permit 90-246**; parallel sign on Pine St. façade and individual letter on Main St. façade. June 1990.
- **Zoning Permit 85-284/COA85-107**; Construct wood canopy over existing entrance stairs and ramp. July 1985.
- **Zoning Permit 79-316**; remove overhang above entrance and across front, repair damage to brick wall. July 1979.
- **Zoning Permit 77-770** to renovate the interior, install exterior door, two new steps, pipe railings, and planter. Change front entrance door swing, erect rear entrance way and metal canopy over side entrance, concrete curb and ramp. April 1977.
- **Zoning Permit issued to Interfaith Senior Citizen Center** to erect a fire escape on the northeast side of the building, install a concrete pad at the same location for a transformer. Grade around foundation and pad. December 1974.

Overview: 86 Main Street includes an existing two story masonry building with an empty lot fronting Main and Pine Streets. This proposal is to explore the erection of a 6 story attached residential/office structure, with minor parking.

Applicable Regulations:

Article 3 (Applications and Reviews), Article 4 (Maps & Districts), Article 5 (Citywide General Regulations), Article 6 (Development Criteria & Guidelines), Article 8 (Parking), Article 9 (Inclusionary and Replacement Housing).

Background Information:

The applicant is seeking sketch plan review for a six story residential / office building on a vacant portion of a corner lot that currently has a 2 story office/service use. Twenty three (23) new residential units are proposed, mostly flats style, but with those on the 5-6 floors townhouse

style. Eight parking spaces will be accommodated on site, with a small office space at street level.

Article 3: Applications and Reviews

Part 3: Impact Fees

Impact fees will be assessed, based upon the new gross floor area of the project.

Part 5, Conditional Use & Major Impact Review

Section 3.5.6 (a) Review Criteria

(a) Conditional Use Review Standards

1. The capacity of existing or planned community facilities;

This proposal was reviewed by the Technical Review Committee (TRC) on July 10, 2014. The applicant was advised to obtain a sewage/location letter from DPW, and to work with the City Stormwater Administrator. No other capacity issues were raised.

2. The character of the area affected;

The Main Street corridor has a mixture of buildings of varying ages, heights and uses. 64-72 Main Street is an enhanced 4 story building; 40-60 Main Street is five floors. 111 Main Street is a four story structure with articulated cornice/roofline that presents a very high building façade. 131 Main Street (Vermont House) is a seven story structure with an eighth floor garden room. The hotel that fronts both Main and (151) St. Paul Streets is being constructed with 4 floors over 2 levels of parking (6 story height.) The majority of buildings fall into the 3-4 story range; but existing character reflects the presence of building heights between 2-7 stories.

3. Traffic on roads and highways in the vicinity;

No traffic information has been provided for this sketch plan review. Traffic information relative to anticipated trip generation and impacts on nearby roadway intersections will be needed when a permit application is filed.

4. Bylaws then in effect;

As assessment of the adequacy of submission information can be made when an application is made. Sketch Plan review typically does not provide sufficient information to determine full compliance with all applicable bylaws.

5. Utilization of renewable energy resources;

No information has been provided with respect to the utilization of renewable energy resources. Although the applicant indicates that no roof access will be provided for tenants, the large flat roofs may be appropriate for solar energy collection.

6. Cumulative impacts of the proposed use;

This criterion requires that cumulative impacts associated with residential development where it is permitted be deemed negligible.

7. Functional family;

Not applicable.

8. Vehicular access points;

A single vehicle access is proposed from Pine Street.

9. Signs;

No signage is included in this proposal. Signs will require separate zoning permits.

10. Mitigation measures;

It is unlikely that the construction of the residential / office structure will require mitigation measures relative to noise or glare within the Downtown district.

11. Time limits for construction;

No special phasing requests for construction have been specified in this sketch plan submission. The zoning permit approval would be valid for 2 years (1 year to start construction and another year to finish).

12. Hours of operation and construction;

Hours of operation are typically not defined for residential development. Office use normally occurs in the daytime, and would not incur special consideration for limitation.

13. Future enlargement or alterations;

In the event of future enlargement or alteration, permits would be required and reviewed under the regulations then in effect.

14. Performance standards;

Performance standards are addressed under Article 5.

15. Conditions and safeguards;

Not applicable for sketch plan.

Section 3.5.6 (b) Major Impact Review Standards

1. Not result in undue water, air, or noise pollution;

No stormwater management details have been provided. A comprehensive stormwater management plan will be required at application. Stormwater management measures should control release into the city system and incorporate infiltration insofar as practicable.

2. Have sufficient water available for its needs;

A letter from the Department of Public Works confirms water and wastewater capacity will be a condition of approval.

3. Not unreasonably burden the city's present or future water supply or distribution system;

See Section 3.5.6 (a) 1.

4. Not cause unreasonable soil erosion or reduction in the capacity of the land to hold water so that a dangerous or unhealthy condition may result;

An erosion prevention and sediment control plan in compliance with Chapter 26, Wastewater, Stormwater, & Pollution Control will be required at application. It will be subject to review and approval by the Stormwater Administrator.

5. Not cause unreasonable congestion or unsafe conditions on highways, streets, waterways, railways, bikeways, pedestrian pathways or other means of transportation, existing or proposed;
This is an existing and fully developed intersection connected to existing transportation infrastructure (highways, sidewalks, bikepath.) The addition of 23 new residential units (most studio or one bedroom) and a very modest office space is anticipated to have a negligible impact on the pedestrian pathways and streets of the City.

6. Not cause an unreasonable burden on the city's ability to provide educational services;
The proposed mixed of studio and single bedroom units in the heart of downtown are unlikely to attract families with small children. It remains possible, however; but required impact fees would help offset any capital costs for children entering Burlington schools.

7. Not place an unreasonable burden on the city's ability to provide municipal services;
The proposed development will generate additional impacts on city services; however, the full extent of those impacts cannot be determined at sketch plan review. City Departments (Parks & Recreation, Fire, Police, Electric, CEDO, Planning & Zoning, School, and Public Works) were invited to participate in technical review of this project. Impact fees will be assessed for the proposed new square footage.

8. Not have an undue adverse effect on rare, irreplaceable or significant natural areas, historic or archaeological sites, nor on the scenic or natural beauty of the area or any part of the city;
None identified. See Section 6.2.2.

9. Not have an undue adverse effect on the city's present or future growth patterns nor on the city's fiscal ability to accommodate such growth, nor on the city's investment in public services and facilities;

The proposed development will take place within the Downtown zone, an area of the city anticipated for increased density. The addition of 23 new residential units and a very small office space should have a negligible impact within the downtown. No adverse impacts on the city's growth patterns are anticipated.

10. Be in substantial conformance with the city's municipal development plan;
In that increased opportunities for residential use within the downtown zone are encouraged by the MDP, as well as consideration of more mixed-use development plans, this proposal conforms to the MDP.

11. Not have an undue adverse impact on the present or projected housing needs of the city in terms of amount, type, affordability and location;

The proposal will provide 23 new housing units within the heart of the downtown, which has been encouraged by both the MDP, most recently by way of Plan BTV: Downtown and Waterfront. Three units will be identified as perpetually affordable (IZ) housing units. In

amount, type, affordability and location, the 23 new residential units will be a welcome addition to the downtown housing mix.

12. Not have an undue adverse impact on the present or projected park and recreation needs of the city.

Modest impacts on the city's park and recreation facilities may result. Payment of impact fees will help offset such impacts.

Article 4: Maps & Districts

Section 4.4.1

(a) Purpose (abridged)

The Downtown Mixed Use districts are intended to provide for a diverse and vibrant mixture of commercial and residential development within Burlington's most highly developed and dynamic urban core.....Residential uses are encouraged on upper floors, but are intended to be secondary to non-residential uses and shall not be allowed to define or dominate the overall development objectives and standards for the district. Active uses are to be provided at street-level in order to create an active and interesting streetscape for pedestrians and enhance the vitality of the downtown area.

Development is intended to be intense with high lot coverage and large tall buildings placed close together. ...Parking is intended to be hidden within, behind, or underneath structures, and parked vehicles should not be visible from the street.

(b) Dimensional Standards & Density

FAR in the Downtown is limited to 5.5, with bonuses possible for additional space. The FAR for the project has been estimated at 3.203, far below the limitation of 5.5

Max lot coverage permissible is 100%, with allowable setbacks of 0. (Although front setback is 12' from curb. There are 2 street frontages within this proposal.) Setbacks from the sidewalk are not annotated on the site plan, and must be to assure the required 12' from curblane.

Height is min. 30' (3 stories) up to 65' max, without bonuses.

Maximum bonus in the Downtown is 8.5 FAR, 105' height with bonuses.

Bonuses are described in (d) 6, below and Section 4.4.1 (d) 7. ÷ (Table 4.4.1-2: Maximum FAR and Building Heights with Bonuses.)

Setbacks for height bonuses are defined in (d) 4. below.

For a description of setbacks, see below.

Proposed lot coverage has been calculated at 85.5%. The maximum permissible is 100%.

(c) Permitted & Conditional Uses

The proposed mixed use is permitted in the Downtown zone. (See Appendix A, Use Table.)

(d) District Specific Regulations

1. Use Restriction

In order to maintain an active streetscape for pedestrians and pedestrian oriented businesses and activities, residential uses shall not be permitted on the ground floor in any structure as follows:

A. Ground Floor Residential Uses Restricted:

i. In the Downtown and Downtown Waterfront district.

No residential uses shall be allowed on the ground floor. As depicted on plans, none are proposed.

2. Public Trust Restrictions

Not applicable.

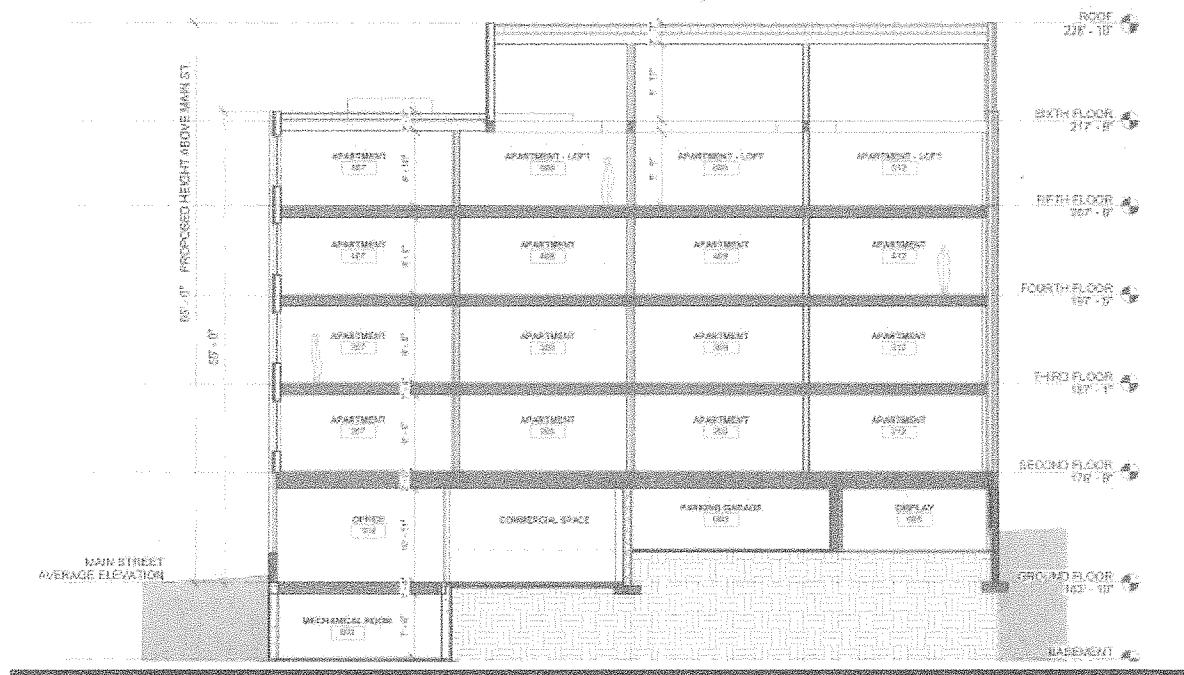
3. Facades and Setbacks on Side and Rear Property Lines

New buildings, or additions or improvements to existing buildings, placed on a side or rear property line where no setback is required may contain neither doors nor windows along such façade. Where the façade of an existing adjacent principal building is within 5 feet of a common property line and has either doors or windows, a setback of 10 feet shall be required for any new development up to the height of the abutting building.

The north side of the proposed building addition has two openings that exit onto an existing 10' right-of-way. It is not clear who owns the land, but the ROW provides the setback required by this section.

4. Building Height Setbacks

A. Principal View Corridors:



Building heights and forms shall respect the principal view corridors, defined as the rights-of-way of Pearl, Cherry, College, and Main Streets, and preserve or enhance views to the lake and mountains. New buildings abutting College and Pearl Streets shall be stepped back above the fourth (4th) story or 45-feet, and new buildings abutting Main and Cherry Streets shall be stepped back above the fifth (5th) story or 55 feet, a distance equal to one-fourth (1/4) the width of the abutting right(s)-of-way from the front property line. In no case shall such upper stories be setback more than forty (40) feet

from the front property line. However, where a principal building with no setback abuts a side lot line, no setback shall be required up to the height of the abutting building.

Elevations illustrate an intended setback from Main Street at 55' height. Main Street's ROW width is 99'. The setback from the front property line for the building above 55' should be at least 25% of that ROW width, not to exceed 40'. Although a required setback is noted on sixth floor plans, the measurement is not annotated. This measurement (minimally 24.75') should be annotated on the plans.

B. Church Street Buildings

Not applicable.

C. Side Street Building Height

As the development site is north of Main Street, not applicable.

5. Lake Champlain Waterfront Setback

Not applicable.

6. Residential District Setback

Parcel does not abut a residential district. Not applicable.

7. Development Bonuses/Additional Allowances

Development bonuses are available for A. Inclusionary Housing, B. Senior Housing, C. Public Parking, D. Job Attraction and Expansions Bonus, E. Green Building, F. Public Art or F. Incorporation of Public Amenities.

H. Maximum Bonus:

<i>In no case shall any development bonuses and allowances granted, either individually or in combination, enable a building to exceed the maximum FAR and maximum building height permitted in any district as defined below: Table 4.4.1-2: Maximum FAR and Building Heights with Bonuses</i>		
Maximum FAR	Maximum Height	
Downtown	8.5 FAR	105 feet

As proposed, the project appears to meet the height (65' max.), FAR (5.5 max.) and setbacks as required by the ordinance without bonuses. No bonuses have been requested.

Article 5: Citywide General Regulations

Section 5.2.3 Lot Coverage Requirements

See Section 4.4.1 (b) above.

Section 5.2.4 Buildable Area Calculation

Not applicable.

Section 5.2.5 Setbacks

See Section 4.4.1 (b) and (d) above.

Section 5.2.6 Building Height Limits

See Table 4.4.1-1, Downtown District or Section 4.4.1 (b), above. Minimum 30', Maximum 65'. As proposed, the building appears to fall within height limitations of the CDO. No bonuses to building height limits are requested.

Section 5.2.7 Density and Intensity of Development Calculations

See Section 4.4.1 (b) above.

Part 4, Special Use Regulations

Section 5.4.8 Historic Buildings and Sites

86 Main Street is not included on the state or National Register of Historic Places; nor has it been identified as archaeologically sensitive. Not applicable.

Section 5.5.1 Nuisance Regulations

Nothing in the proposal appears to constitute a nuisance under this criterion.

Section 5.5.2 Outdoor Lighting

No outdoor lighting information has been provided. A lighting plan meeting the standards of this section will be required as part of a formal application for review.

Section 5.5.3 Stormwater and Erosion Control

As noted previously, no stormwater management details have been provided. A comprehensive stormwater management plan will be required at the time of application.

Section 5.5.4 Tree Removal

The site of the building addition is currently pavement. No trees are proposed for removal.

Article 6: Development Review Standards:

Part 1, Land Division Design Standards

Not applicable.

Part 2, Site Plan Design Standards

Section 6.2.2 Review Standards

(a) Protection of important natural features

There are no important natural features on the subject property. The site is almost entirely impervious/pavement. Guidance from the City Arborist should be sought as to the appropriate landscaping or any street trees to be included within the project area.

(b) Topographical alterations

There are existing grade changes along both Main Street and Pine Street at the development site. Excavation is proposed for the lower level/mechanical area; however the finished grade is anticipated to remain the same.

(c) Protection of important public views

East/west views as observed on Main Street will remain. The required setback in the Downtown above 55' in height will retain those public views toward the lake.

(d) Protection of important cultural resources

The property is not included in the city's map of archeologically or architecturally sensitive areas. Not applicable.

(e) Supporting the use of alternative energy

No information has been provided regarding the use of alternative energy. The use of wind, solar, water, geothermal, or other alternative energies is broadly encouraged.

(f) Brownfield sites

None identified.

(g) Provide for nature's events

See Section 5.5.3.

(h) Building location and orientation

The new building will be aligned along the street edge with the existing 2 story building; reinforcing the street and continuing around the corner to Pine Street. Spatially and architecturally the project will strengthen this corner. This has been a particularly "blank hole" which will now have a presence and street edge.

Building openings are proposed along Main Street, and a garage entrance opening on Pine Street. The broad expanse of glazing on both street facades opens the building up visually; however a pedestrian entrance on Pine Street would be welcome.

(i) Vehicular access

One vehicular access point, from Pine, will be provided into the site. A curb cut exists under current conditions but will likely be modified to accommodate the proposed development. Adequate access for emergency vehicles and fire apparatus will be required.

(j) Pedestrian access

Pedestrian access to the new building is available from the public sidewalk via two entrances off Main Street. Building residents may enter via the ROW off Pine Street on the rear/north elevation as well.

(k) Accessibility for the handicapped

Main Street entrances will allow access to an elevator for upper residential floors. There is an existing ramp at the rear of the building, although it is not clear how this may benefit the new building. A handicap parking space is identified within the small garage area, although it does not appear to have a circulation path to the elevator but be limited to the h/c ramp. It will be important to explore how a mobility impaired resident might park their vehicle and maneuver to an upper floor residence.

The building inspector will require ADA compliance for access, and visitability standards for the remainder of the project.

(l) Parking and circulation

Parking is limited to 8 internal spaces. The remainder of the required parking is proposed to be leased off-site, at the 40 Main Street parking garage where the project owner is a leasee.

See Article 8 for a discussion of parking requirements.

A comprehensive parking analysis will have to be provided at the time of application to assess the adequacy of the parking and management plan.

(m) Landscaping and fences

None articulated. This will need to be included within the application.

(n) Public plazas and open space

No public plazas or open space are depicted on plans.

(o) Outdoor lighting

See Section 5.5.2.

(p) Integrate infrastructure into the design

No infrastructure details have been provided. All utility lines must be buried. Mechanical equipment (such as HVAC), trash, and recycling facility details must be provided at the time of application. Any mechanical equipment must be integrated within the design envelope rather than added as an afterthought.

Part 3, Architectural Design Standards

Section 6.3.2 Review Standards

(a) Relate development to its environment:

1. Massing, Height and Scale:

See Section 3.5.6 (a) 2. Main Street has a variety of building heights; most falling within the 2-7 story range. As this building is proposed to be six stories (set back at the fifth floor/55'), it will not be incongruous within the streetscape of larger buildings in the downtown core.

The building as designed engages street level interest with ample glazing; which in turn amplifies the verticality of the design. The arrangement avoids monotony with a slightly off-center structural "canvas" that energizes and activates the primary façade. The pine Street façade provides a pedestrian level ribbon of glass, with the vehicular access entry. Again, the arrangement of cladding provides interest and detail to the overall design.

2. Roofs and Rooflines.

Flat roofs are proposed for both building volumes. This is common in high rise buildings and those located within the downtown core.

Solar panels, light colored ballast or roof membranes, split roof clerestories, planted or "green" roof technologies (with a clearly articulated maintenance plan) and "gray water" collection are encouraged. Active rooftop uses are also encouraged to add to the visual complexity and activity of the city's skyline, and afford public access to otherwise unseen views of the city and surrounding landscape.

The applicant indicated that roof access is not proposed for the residents. Solar is encouraged.

3. Building Openings

Principal entrances shall be clearly defined and readily identifiable from a public street whether by a door, a canopy, porch, or other prominent architectural or landscape features. People with physical challenges should be able to use the same entrance as everyone-else and shall be provided an “accessible route” to the building. Attention shall also be accorded to design features which provide protection from the affects of rain, snow, and ice at building entrances, and to provisions for snow and ice removal or storage.

Principle entrances are provided on the Main Street level, and two entrances to the parking level from the rear (north) ROW. Vehicular access is limited to Pine Street.

Handicap access is provided for the existing two story building via the ramp on the north. See comments above (Section 6.2.2. (k)) about h/c access between the parking level and residential spaces above.

Window openings shall maintain consistent patterns and proportions appropriate to the use. The window pattern should add variety and interest to the architecture, and be proportioned to appear more vertical than horizontal. Where awnings over windows or doors are used, the lowest edge of the awning shall be at least eight (8) feet above any pedestrian way, and shall not encroach into the public right-of-way without an encroachment permit issued by the dept. of public works.

Window openings are consistent in arrangement yet so designed as to avoid monotony. They lead the eye upward to articulate the building volume in a pleasant rhythm.

No awnings are articulated on the new building. Some consideration is recommended for a roof cover or overhang at the Main Street entrance to protect users from inclement weather. Such a canopy appears at the existing building entrance. A new canopy or other similar feature will require an encumbrance permit if it reaches into the public street right-of-way.

Buildings placed on a side or rear property line where no setback is required shall contain neither doors nor windows along such façade so as not to restrict future development or re-development options of the adjacent property due to fire safety code restrictions. Otherwise they should be setback a minimum of 5-feet.

This building is proposed to be attached to the existing 2 story building, so the only boundary line it approaches is on the north (Pine Street) side. The site plan identifies an existing 10' ROW. It is not clear who owns that ROW, but as presented, the building meets the required setback for abutting property.

(b) Protection of Important Architectural Resources:

See Section 6.2.2. (d).

(c) Protection of Important Public Views:

See Section 6.2.2. (c).

(d) Provide an active and inviting street edge:

Building facades shall be varied along the street edge by the integration of architectural features, building materials, or physical step-backs of the façade along its length. Large

expanses of undifferentiated building wall shall be avoided. This may be accomplished by incorporating fenestration patterns, bays, horizontal and vertical façade articulations, the rhythm of openings and prominent architectural features such as porches, patios, bays, articulated bases, stepping back an elevation relative to surrounding structures, and other street level details. The use of traditional facade components such as parapet caps, cornices, storefronts, awnings, canopies, transoms, kick plates, and recessed entries are highly encouraged. In areas where high volumes of pedestrian traffic are desired, the use of architectural recesses and articulations at the street-level are particularly important in order to facilitate the flow of pedestrian traffic.

As noted, pedestrian interest is engaged with an almost continuous expanse of glass along the entire streetfront elevation along Main and Pine Streets. The integration of differing building materials (brick, aluminum, and metal panels) activates the building elevations and provides an announced modern avowal. The attachment to a 1950s commercial building provides a curious yet compelling and pleasant counterpoint.

There do not appear to be any building recesses or planar changes along either streetfront, however the building elevations seem to provide shadow lines that suggest them. If part of the proposal, they would enhance the building's dimensional interest.

The applicant is cautioned to provide activity *within* the glazed areas to assure street interest, rather than reflective panels or dormant display areas. To truly engage and retain the interest along the street, there should be activity behind the glass. See next standard for further direction.

Non-residential buildings should provide visual access into the interior of building at the street level through the use of large transparent windows and/or window displays in order to create a dynamic and engaging public streetscape. The use of mirrored, frosted, or tinted glass shall not be permitted along an active pedestrian street-level façade. In contrast, residential buildings may be slightly recessed and/or elevated from the street-level in order to provide privacy. In such cases, visual interest along the streetscape can be provided through the use of landscaping, porches, and other similar features that offer a transition between public and private space.

Buildings in downtown districts that provide open space by way of building setbacks at the ground level shall utilize landscaping, street furniture, public art, sitting walls, fountains, etc. to maintain a sense of the existing street wall, define a sense of entry for the building and create a space that enhances the pedestrian's experience. Urban "open" space shall maximize accessibility for all individuals including the disabled, and encourage social interaction.

The building continues the existing "streetedge" with its immediate attachment to the existing two story structure. An entrance lobby is accessible from Main Street, which in turn provides privacy for the residential units on floors 2-6. As the building entrance is not recessed, street furniture or sitting walls would not seem appropriate here.

This primary entrance will be handicap accessible; an elevator is available within the lobby area.

(e) Quality of materials:

All development shall maximize the use of highly durable building materials that extend the life cycle of the building, and reduce maintenance, waste, and environmental impacts. Such materials are particularly important in certain highly trafficked locations such as along major streets, sidewalks, loading areas, and driveways. Efforts to incorporate the use of recycled

content materials and building materials and products that are extracted and/or manufactured within the region are highly encouraged.

The proposed exterior materials are primarily brick, aluminum storefront and insulated metal panels. All are anticipated to be durable and able to withstand the local harsh climate.

(f) Reduce energy utilization:

New structures should incorporate the best available technologies and materials in order to maximize energy efficient design. All new construction shall meet the Guidelines for Energy Efficient Construction pursuant to the requirements of Article VI. Energy Conservation, Section 8 of the City of Burlington Code of Ordinances.

Conformance with current energy efficiency standards, as defined by Burlington Electric, will be required.

New structures should take advantage of solar access where available, and shall undertake efforts to reduce the impacts of shadows cast on adjacent buildings where practicable, in order to provide opportunities for the use of active and passive solar utilization.

As noted, solar is encouraged. At the very least, infrastructure to allow for future solar installation should be considered within the design envelope.

A shadow study should be explored for the DRB submission. Preliminary analysis for little negative shadow impact, based on the abutting street (east) and parking area (north) may be substantiated with further modeling.

(g) Make advertising features complementary to the site:

No signage is proposed. Any signs will require a separate sign permit.

(h) Integrate infrastructure into the building design:

See Section 6.2.2. (p).

(i) Make spaces secure and safe:

All requirements for ingress and egress, as defined by the building inspector and fire marshal shall be implemented. Consideration of appropriate lighting at building entrances and within the parking area shall be explored, especially relative to resident safety. Building should be sprinklered per standards defined by the fire marshal. Intercom systems for multi-family housing should be incorporated where possible, to maximize personal safety.

Article 8: Parking

Section 8.1.8 Minimum Off-Street Parking Requirements

The subject property is located in the Downtown parking district. As a result, each dwelling unit requires 1 parking space for a total of 23 spaces. The sketch plans define 8 interior parking spaces. The balance of required parking is intended to be leased off-site at 40 Main Street.

There are existing uses in the attached building (hair salon, medical offices) which have parking requirements. An examination of previous permitting does not define specific requirements at the time of permit issuance. (ZP11-0524CA; 1030 sq. ft. of medical office; ZP10-0273CA; 3324

sq. ft., or entire 1st floor for salon with retail.) By current parking requirements, the medical office on the 2nd floor would require 1 parking space (1/1000 sq. ft.); and the salon would require 7 parking spaces (2/1,000 sq. ft.). The total parking requirement for those existing uses would be 8 parking spaces, which currently exist in the parking lot and are replicated in this application as the interior parking spaces.

The proposed new general office will need 2 spaces/ 1000 sq. ft., or 1 parking space for the 492 sq. ft. space.

For the new building: Residential = 23 parking spaces

Office = 1 parking space

A comprehensive analysis of the parking requirement for all uses and provisions for meeting CDO standards will be required for application.

Section 8.2.5 Bicycle Parking Requirements

Table 8.2.5-1, Bicycle Parking Requirements:

Residential, household living

Multi unit: 1 per 4 units long term spaces; 1 per 10 units short term spaces.

For 23 residential units, **6 long term** (23/4) and **2 short term** (23/10) must be provided.

For offices:

1/5,000 sq. ft. Long Term spaces; 1 per 10,000 sq. ft. short term spaces. For a 492 sq. ft. office, no bicycle parking would be required, but would be encouraged.

As noted in overall parking requirements, a comprehensive assessment of all uses on the site and requirements for bike parking will need to be submitted at the time of application.

The bike parking spaces must comply with the Department of Public Works' Bicycle Parking Guidelines.

Article 9: Inclusionary and Replacement Housing

Section 9.1.5 Applicability

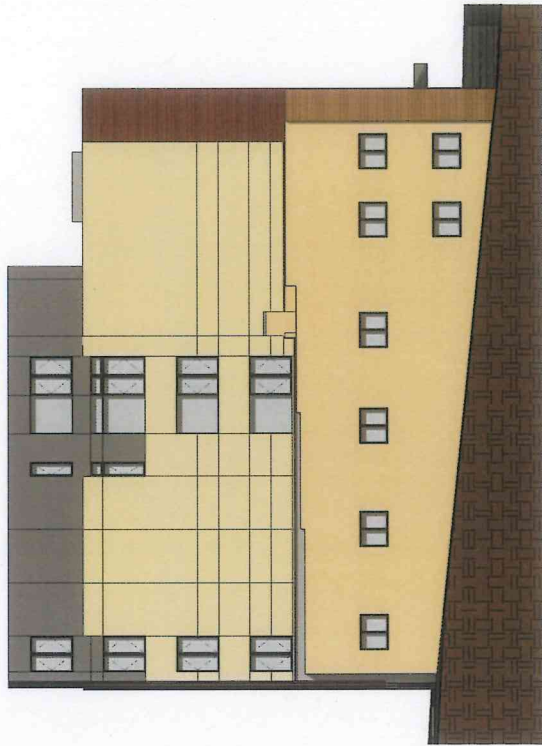
As the proposed development includes more than 5 new dwelling units, it is subject to the inclusionary housing provisions of this Article. Fifteen percent of the new residential units must be inclusionary (23 units are proposed $\times .15 = 3$ units IZ). Impact Fees may be reduced for the inclusion of affordable units. A letter of compliance from the Housing Trust Fund manager will be required to define any reduction in the amount of impact fees to be assessed, based upon the square footage of new inclusionary units.

NOTE: These are staff comments only. The Development Review Board, who may approve, table, modify, or deny projects, makes decisions.

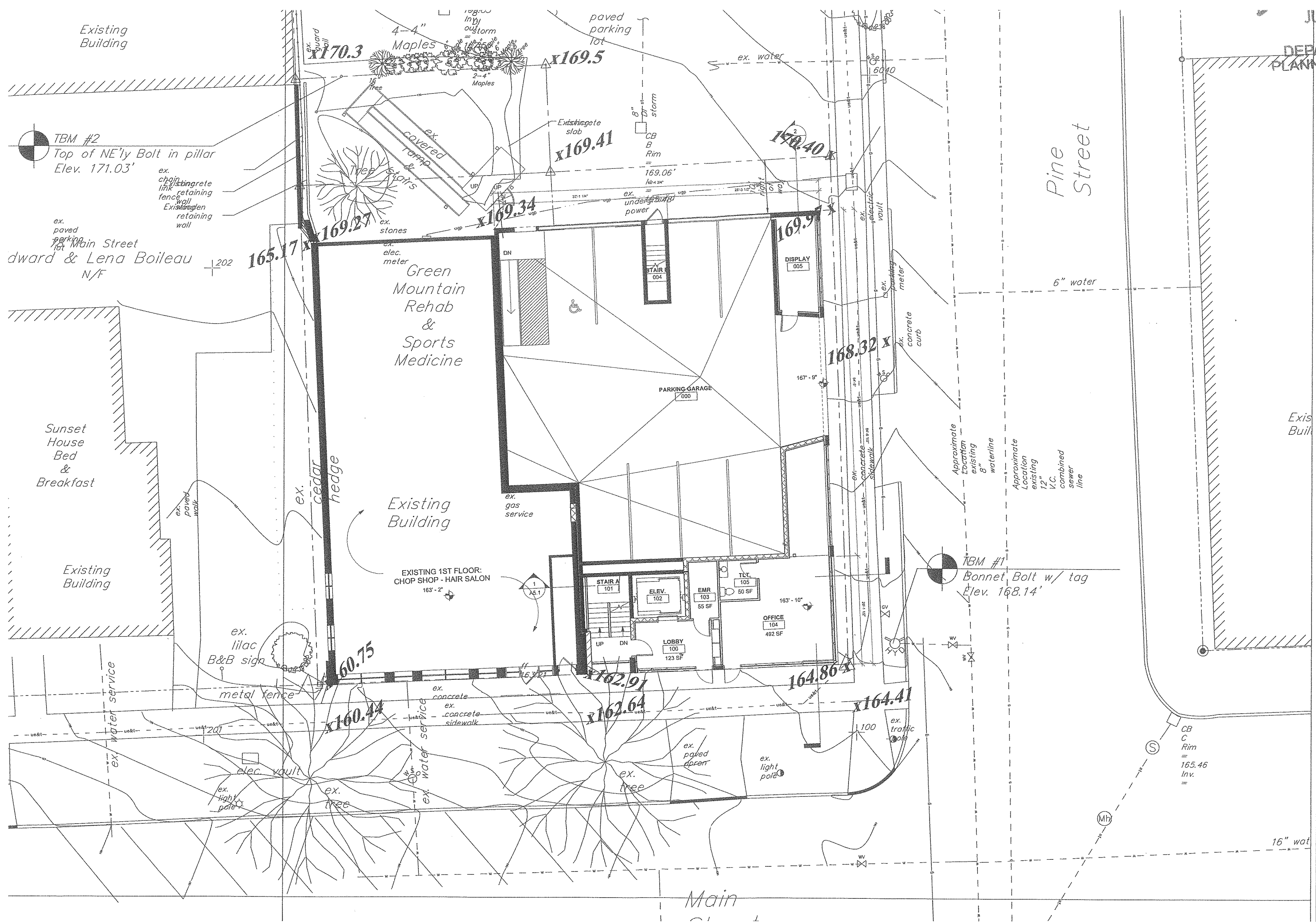
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SCOTT + PARTNERS ARCHITECTURE 20 MAIN ST., SECOND FLOOR, BURLINGTON, VT 05402 P. 802.475.1313 F. 802.475.1314 SCOTTMAIN@SCOTTMAIN.COM		Date Revisions		



① NORTH ELEVATION
1/8" = 1'-0"



② WEST ELEVATION
1/8" = 1'-0"



JUN 27 2014

DEPARTMENT OF
PLANNING & ZONING

KREBS & LANSING
CONSULTING
ENGINEERS, INC.
164 MAIN STREET
COLCHESTER, VT
05446

SCOTT + PARTNERS
ARCHITECTURE

20 MAIN ST. ESEX JUNCTION, VT 05452
P. 802.879.5153 F. 802.872.2764 SCOTTPARTNERS.COM

86 MAIN STREET
APARTMENTS
BURLINGTON, VT

project name:

scale: 1/8" = 1'-0"

project no. 13-955

checked by: TS

drawn by: JM

date: 6/27/2014

date	revisions

sheet title:

SITE
PLAN

sheet no.

A24



RECEIVED

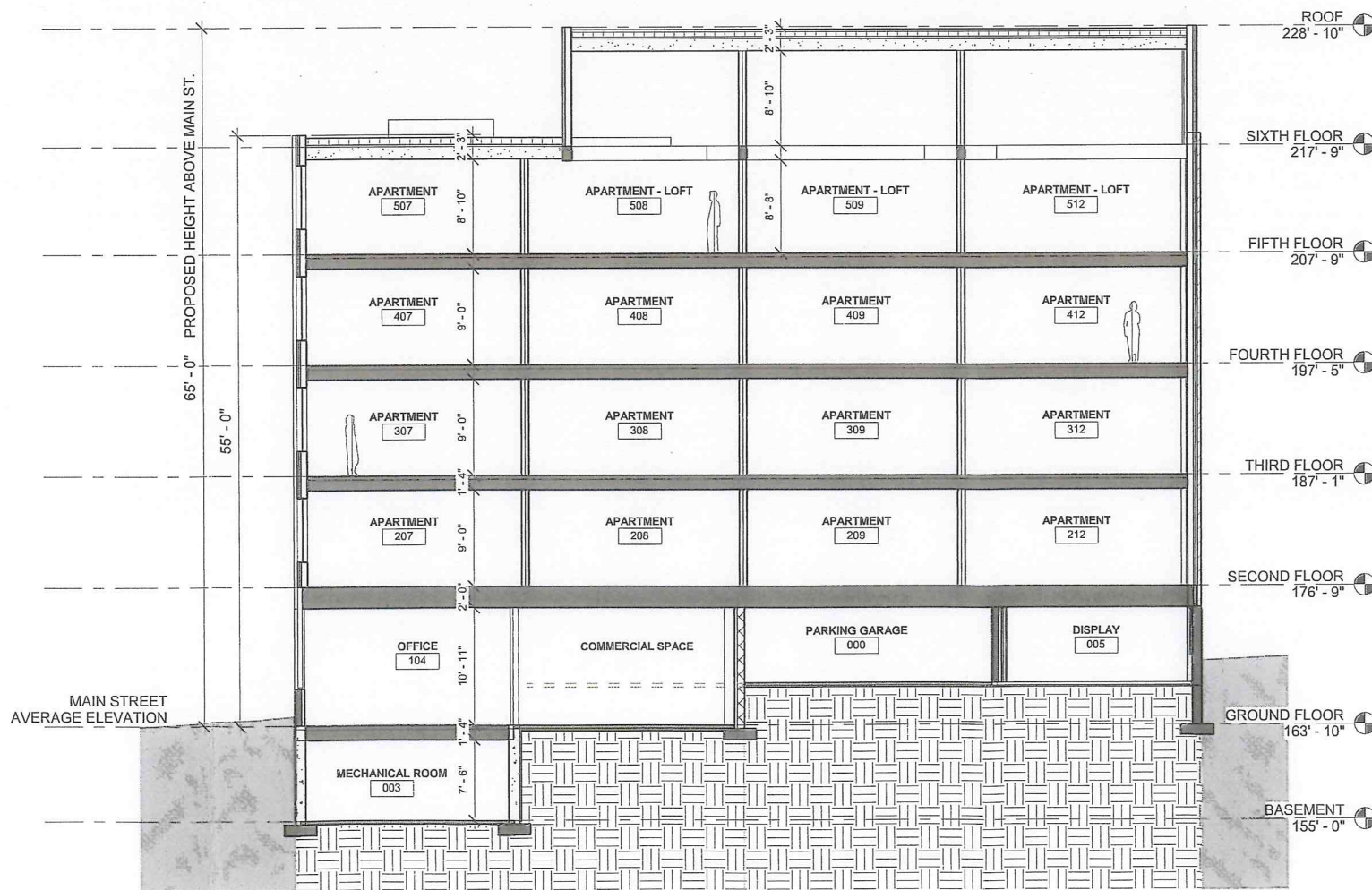
JUN 27 2014

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SCOTT + PARTNERS
ARCHITECTURE

20 MAIN ST. ESSEX JUNCTION, VT 05452
P. 802.879.5153 F. 802.872.2764 SCOTTPARTNERS.COM



Project name:

86 MAIN STREET APARTMENTS

BURLINGTON, VT

scale: 1/8" = 1'-0"
project no. 13-955
checked by: Checker
drawn by: Author
date: 6.26.2014

Date Revisions

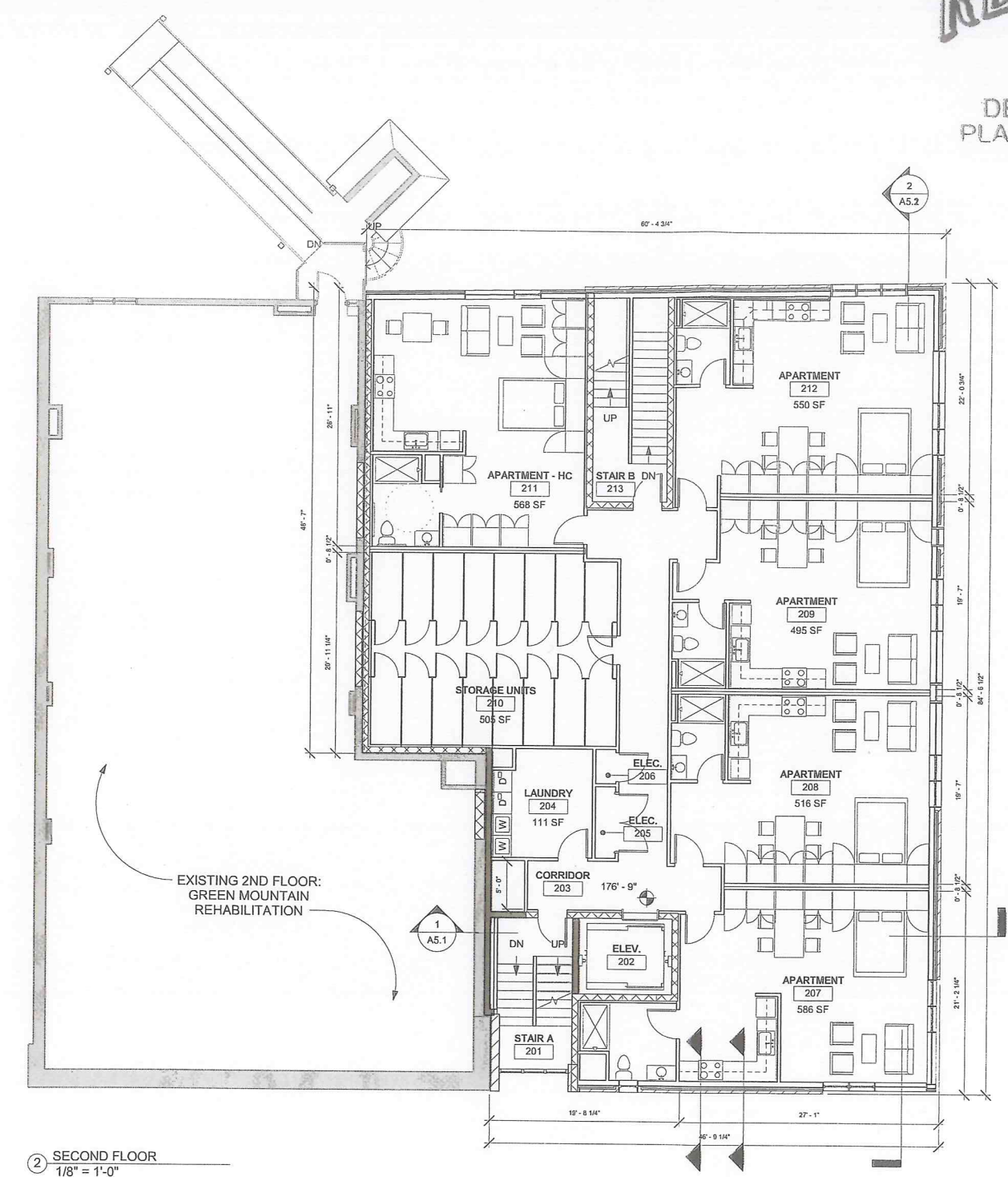
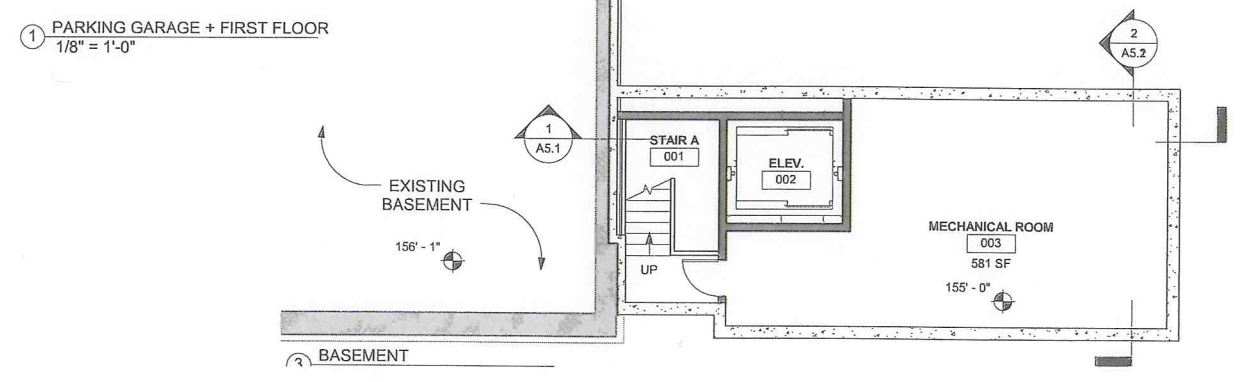
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OPTION A
EAST SECTION &
STREET FACADES

sheet no.
A E 2

DEPARTMENT OF
PLANNING & ZONING

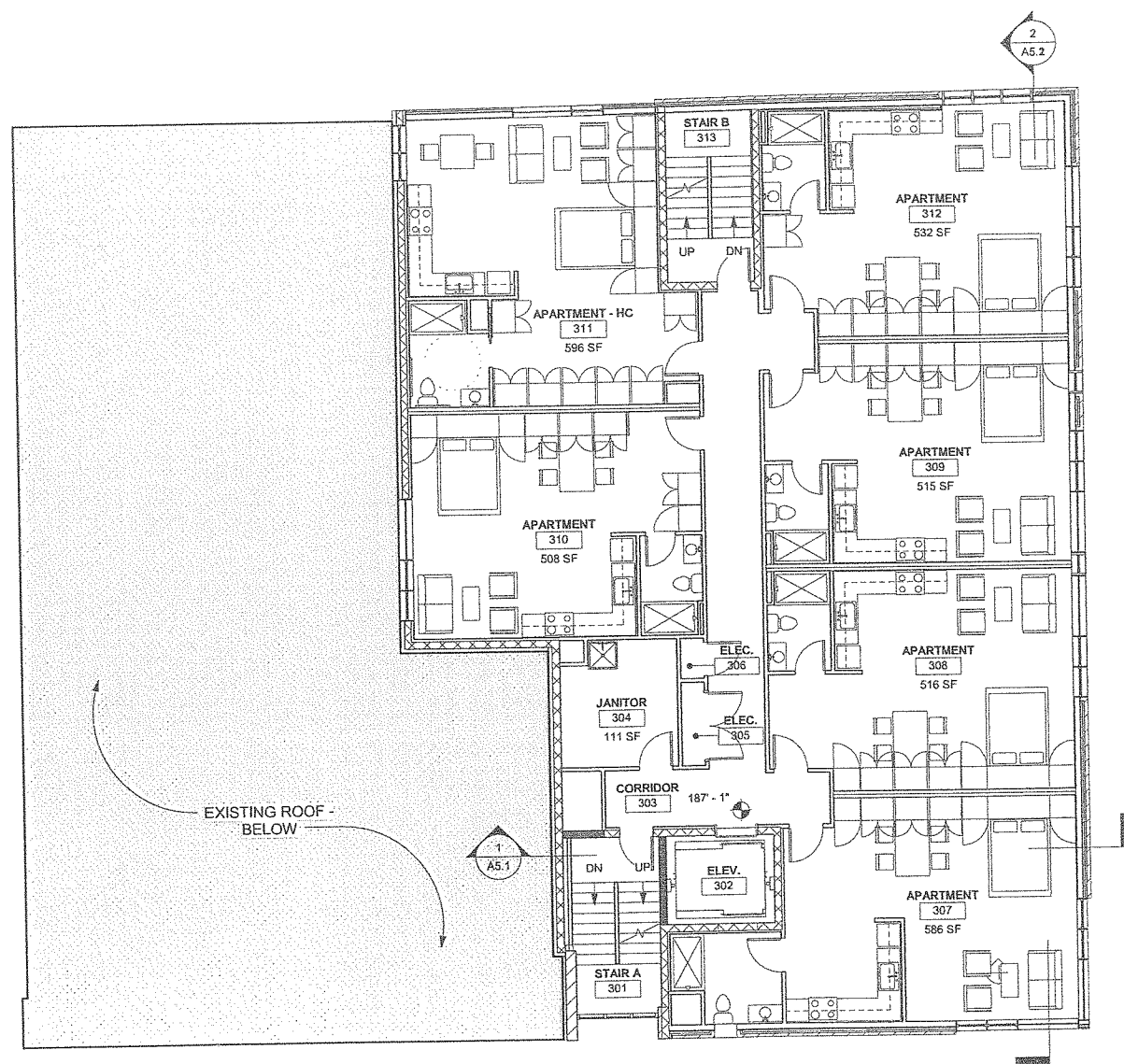
86 MAIN STREET APARTMENTS
BURLINGTON, VT

sheet no.



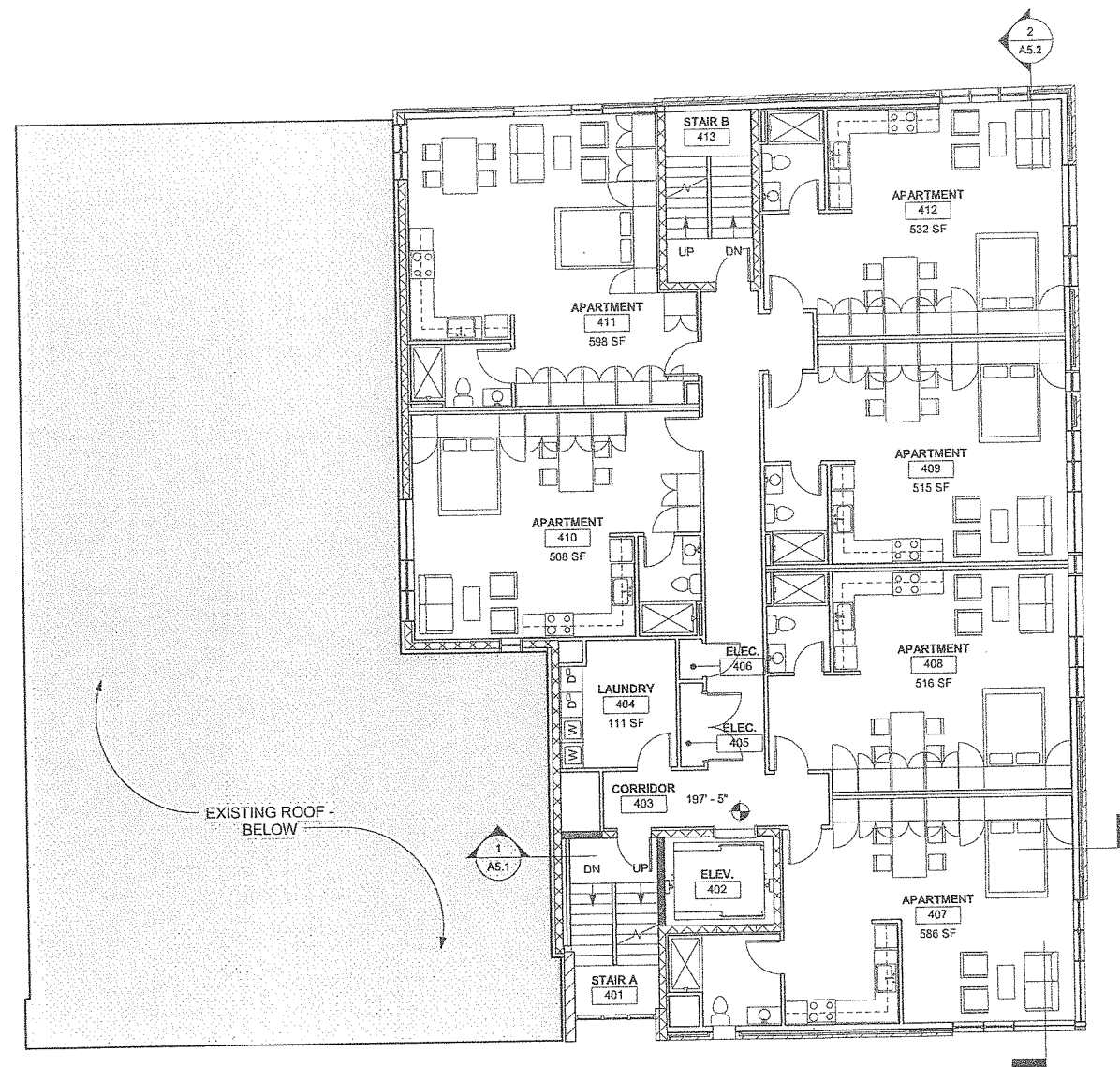
JUN 27 2014

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① THIRD FLOOR
1/8" = 1'-0"

NOTE:
REFER TO SECOND FLOOR FOR DIMENSIONS.



③ FOURTH FLOOR
1/8" = 1'-0"

NOTE:
REFER TO SECOND FLOOR FOR DIMENSIONS.

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ARCHITECTURE
20 MAIN ST. ESSEX JUNCTION, VT 05402
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project name:
86 MAIN STREET APARTMENTS
BURLINGTON, VT

scale: 1/8" = 1'-0"
project no. 13-95
checked by: JT
drawn by: JC
date: 6.27.2014

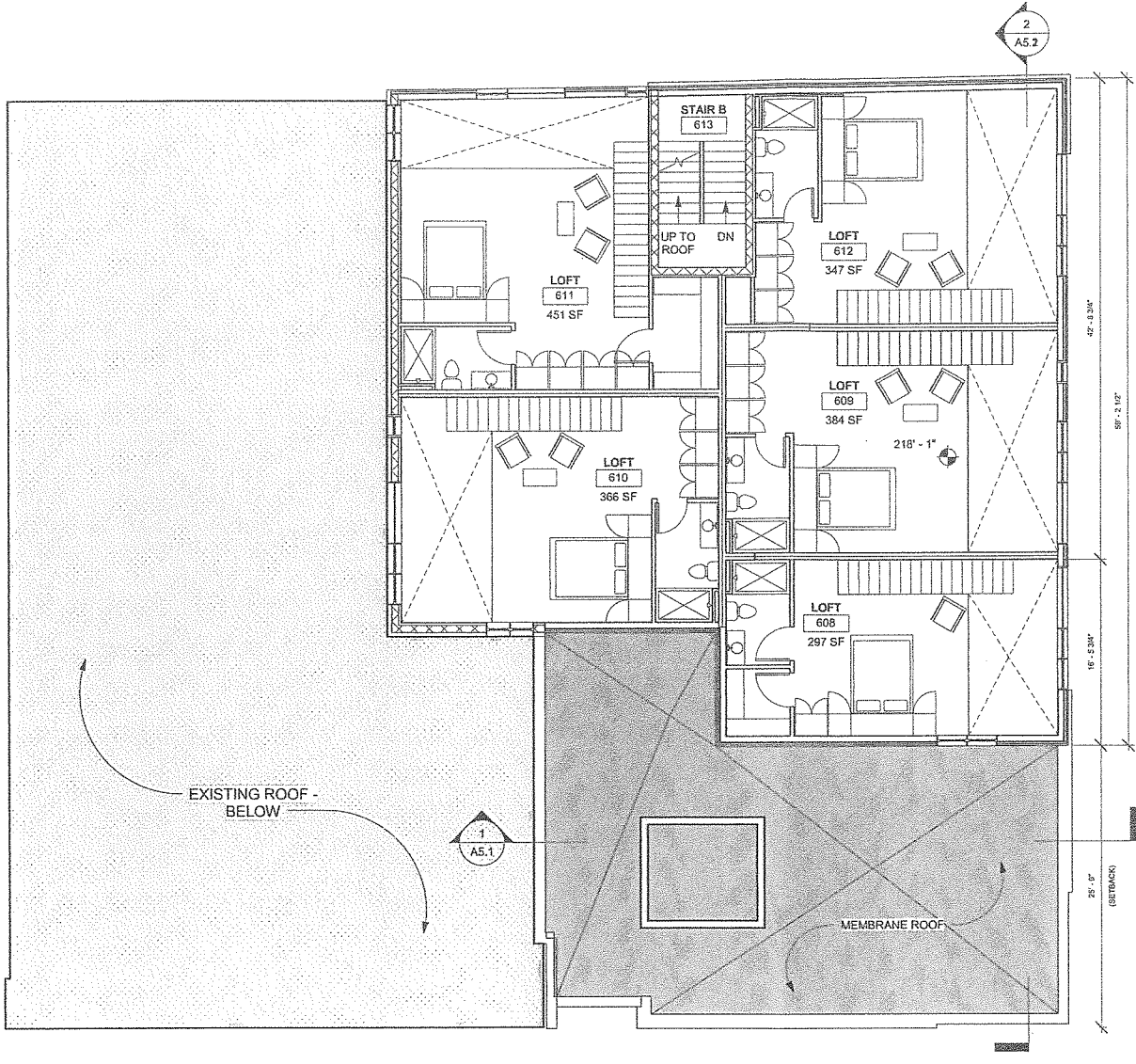
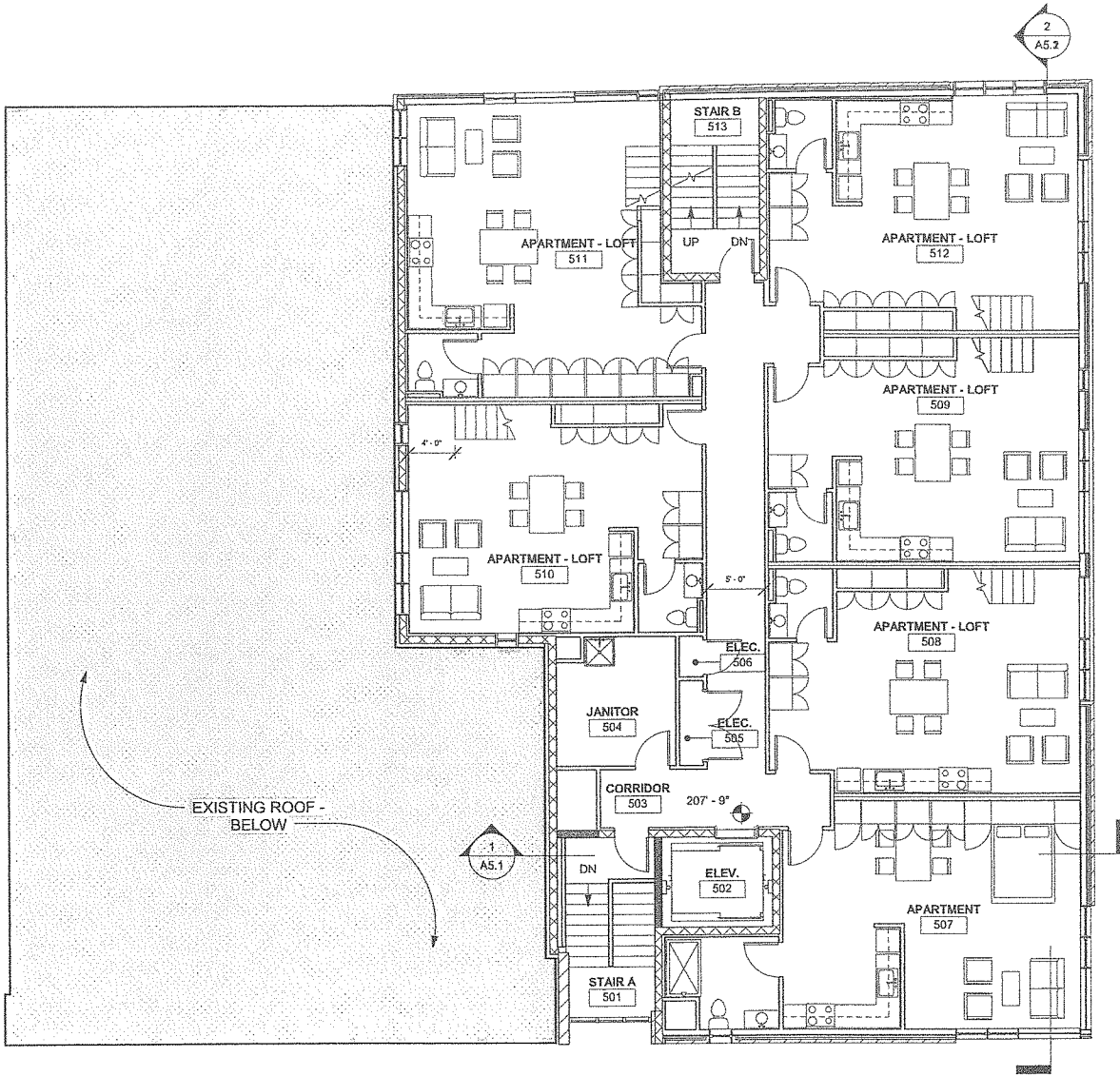
Date	Revisions
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sheet title:
THIRD + FOURTH
FLOOR PLANS

sheet no.
A2.2

JUN 27 2014

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① FIFTH FLOOR
1/8" = 1'-0"

② SIXTH FLOOR
1/8" = 1'-0"

NOTE:
REFER TO SECOND FLOOR FOR DIMENSIONS.

SCOTT + PARTNERS
ARCHITECTURE

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project name:
86 MAIN STREET APARTMENTS
BURLINGTON, VT

scale: 1/8" = 1'-0"
project no. 13-955
checked by: TS
drawn by: JG
date: 6.27.2014

Date	Revisions
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sheet title:
**FIFTH + SIXTH
FLOOR PLANS**

sheet no.
A2.3

